

CHAPTER 3: LAND USE

3.1 Purpose

The Land Use Plan establishes the direction for guiding development and redevelopment in the City. The plan provides the framework for land use decisions through 2040. The Land Use Plan is meant to clearly communicate the City's desired vision of land uses for development and redevelopment to ensure a sustainable future as a healthy vibrant City. The Land Use Plan is not intended to be a static document. It shall be reviewed and revised when necessary to incorporate changing conditions and community goals. Implementation of the Land Use Plan will be carried out through the official controls included in the City's Subdivision and Zoning Ordinances.

3.2 Influencing Factors

A variety of factors influence land uses in the City including the following, which are discussed in this section: the natural environment, transportation network, surrounding communities and development staging for undeveloped property in the City. This section provides a broad overview of issues that affect land use in the City. Sections 3.3 and 3.4 of this chapter provide a more detailed analysis of land uses.

3.2.1 Existing Natural Environment

Over 90 percent of Brooklyn Park is developed, however, natural features still influence land uses in the City. The following section provides an inventory of water, topography and woodlands in the City.

Water: Channels, Creeks, Lakes, Ponds, Rivers.

Map 3-1 shows the water features in Brooklyn Park as mapped according to surveys and aerial photography of the City.

Channels and Creeks. Three creeks run through Brooklyn Park: Bass Creek in the southwest corner, Eagle Creek also in the southwest corner, and Shingle Creek in the south. Mattson Brook is the name for only the eastern portion of the creek which flows through the center of Brooklyn Park. Mattson is more of a channel or drainage way than a traditional creek.

Lakes. The City contains three lakes: Lake Magda in the southwest corner, Lake Success in central Brooklyn Park and Oxbow Lake in the north. Of these lakes, only Lake Magda is natural. Changes have occurred in Brooklyn Park's watertable and groundwater in the past decade. More information regarding the water table can be found in Chapter 6: Utilities.

Storm Water Management Basins. Infiltration basins or ponds are scattered throughout Brooklyn Park and are based on Engineering Department Global Positioning System (GPS) surveys and aerial photography of the City. The MN Department of Natural Resources (DNR) provides some wetland information (Reference archive map 1982). Wetlands are not completely accurate in Map 3-4 because of changing conditions. Unpredictable natural factors affect the shape of wetlands such as annual rainfall and the water table. There are also several classification systems for wetlands maintained by varying entities such as: Army Corp of Engineers, MN Pollution Control Agency, MN Department of

Natural Resources and Watershed Districts. The City continues to monitor ponds and wetlands through the Engineering Department, Watershed District and DNR review.

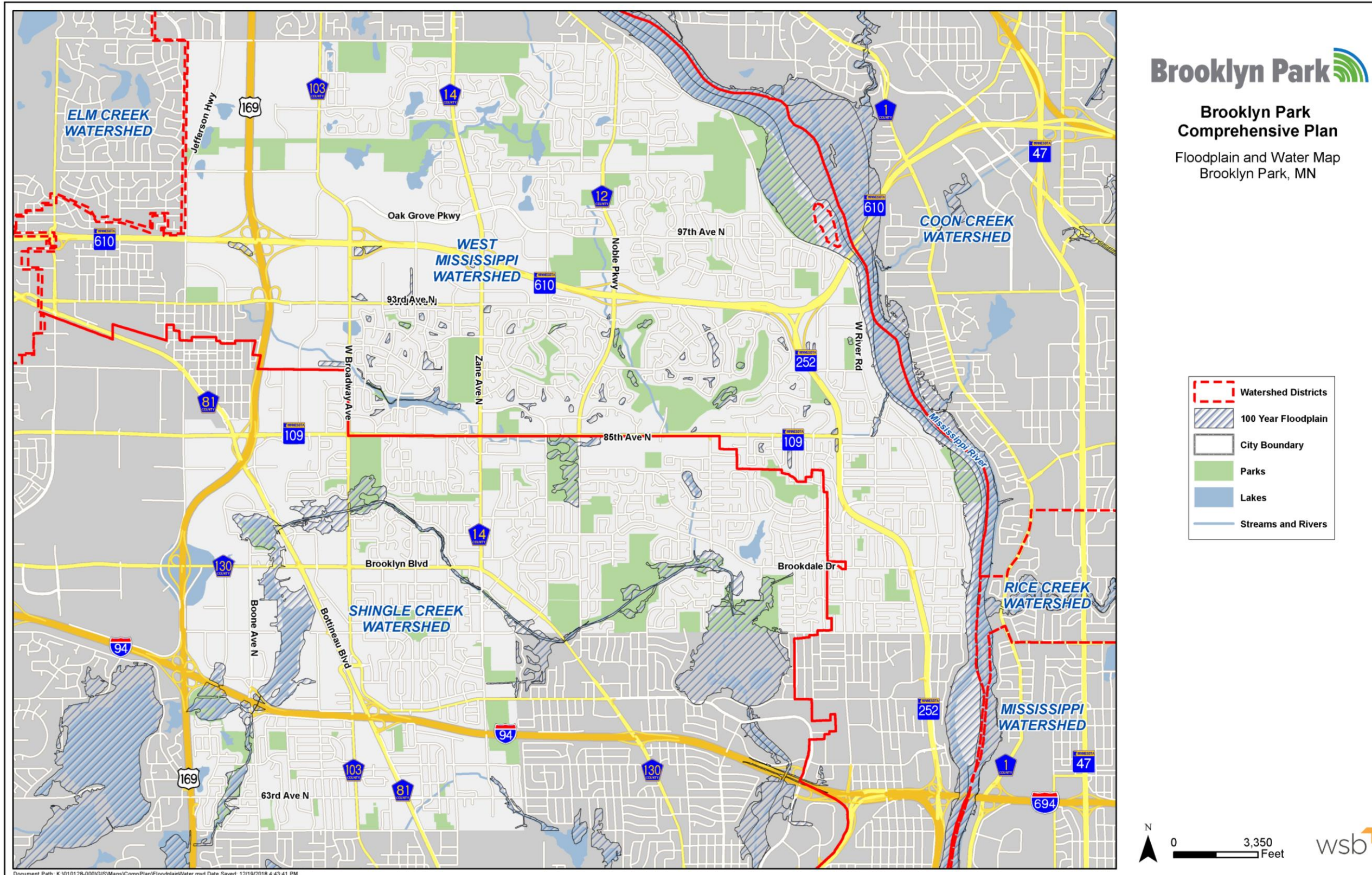
River. The Mississippi River is Brooklyn Park's eastern border and acts as the City's primary water feature. Small islands exist on the River, among them Durnam, Banfill and Dunn. The above natural resources are major amenities for the citizens of Brooklyn Park and this plan calls for the preservation of water features as amenities.

Watersheds

There are two watersheds in Brooklyn Park: Shingle Creek watershed in the south and West Mississippi watershed in the north. Development proposals affecting the watersheds are reviewed by the corresponding Commissions. The Shingle Creek watershed is managed by the Shingle Creek Watershed Management Commission which is governed by a nine-member board comprised of representatives from each member city who are appointed for terms of three years. The nine member cities are Brooklyn Center, Brooklyn Park, Crystal, Maple Grove, Minneapolis, New Hope, Osseo, Plymouth and Robbinsdale.

The West Mississippi watershed is managed by the West Mississippi Watershed Management Commission that is governed by a five-member board comprised of representatives from each member city who are appointed for terms of three years: The five member cities are Brooklyn Center, Brooklyn Park, Champlin, Maple Grove and Osseo. The following map illustrates the watershed districts and water features in Brooklyn Park.

MAP 3-1 FLOODPLAIN AND WATERSHEDS



Topography

The Topography information in [Map 3-2](#) was mostly collected from the Minnesota Department of Natural Resources Data online. The data reveals that the vast majority of Brooklyn Park is level except some areas of steep slopes (greater than 12%) along the Mississippi River, W. River Rd. and Highway 610.

Woodlands

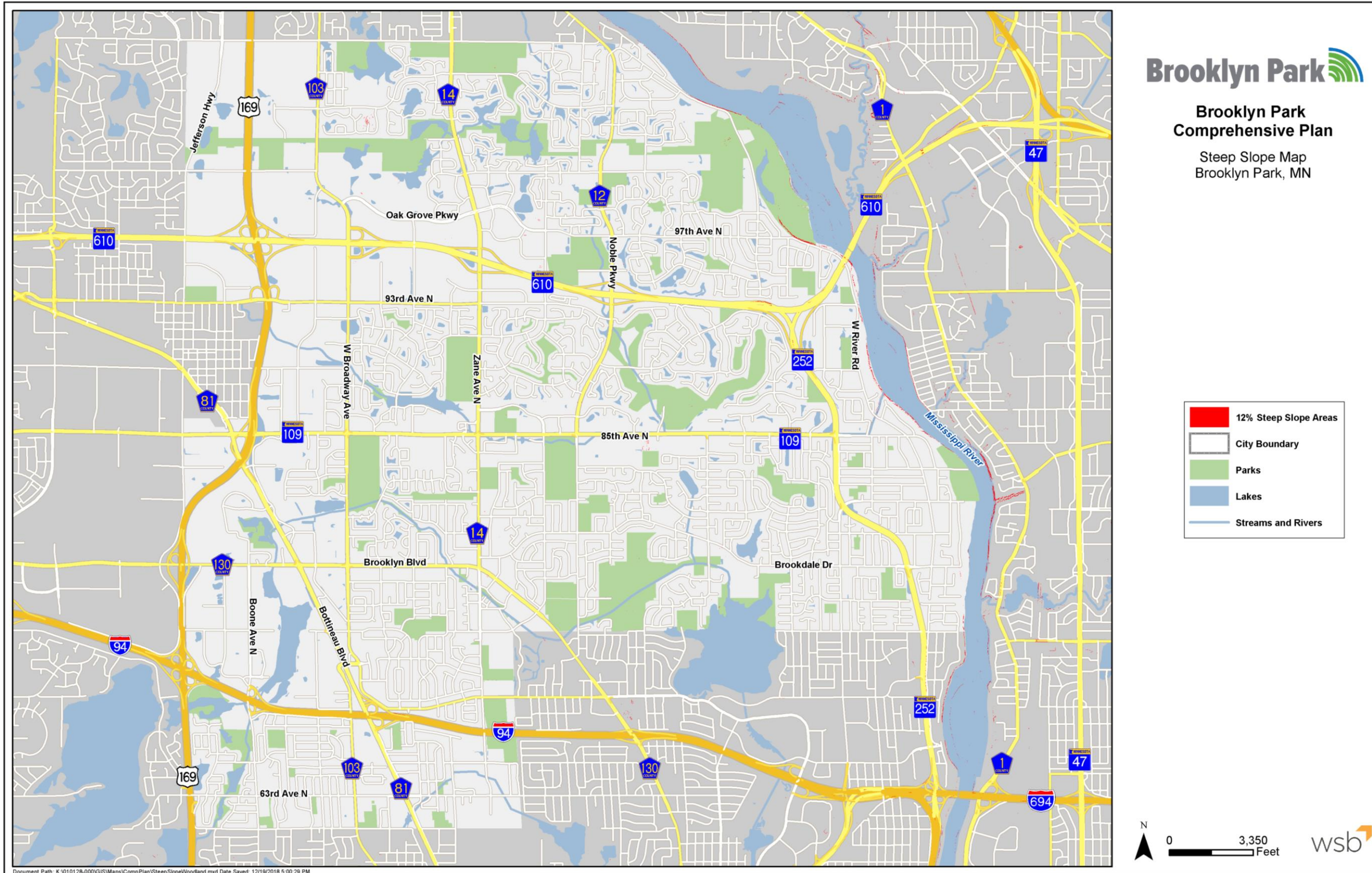
Trees are relatively scarce in Brooklyn Park. The major stands occur in two locations: 1) Along the creeks, wetlands and river and 2) in the City and County Parks. Species in the stands include aspen, poplar, maple, elm, basswood and some oaks. In May of 2001 Bonestroo, Rosene, Anderlik and Associates finished a detailed Natural Resources Inventory of the woodlands and wetlands in the northern growth area.

The inventory identified two areas of woodlands rated as “good” in quality. The first area is the property east of Winnetka at the 103rd / 104th Avenue alignment) The second area is the Schreiber property north of Oak Grove Parkway (97th) and west of Douglas Drive. In developing the Schreiber property, special consideration was given to preserving the woodlands on the property. A portion of the woods was preserved through development and is now owned by the City.

In 2002 a third woodland was identified by The Izaak Walton League of America. The woodland is about 5-6 acres of oak savanna along Jefferson Highway (pictured below). The Walton League estimates the age of some of the oaks to be around 200 years old. The oaks are located on the Smith property at 10609 Jefferson Hwy. In order to ensure the woods on the Smith property is saved this plan identifies the site for conservation.



PHOTO 3.2.1: OAK SAVANNA ALONG JEFFERSON HIGHWAY (SPRING 2007).



3.2.2 Transportation Network

Several metropolitan highways provide Brooklyn Park with access to the Twin Cities metropolitan area and to Greater Minnesota. These roadways include Interstate 94/694 in the southwestern portion of the City; Trunk Highway 169 near and along the western edge of the City; Trunk Highway 252 across the eastern portion of the City, and Trunk Highway 610, located in the northern portion of the City. Currently the City is preparing for development of the METRO Blue Line Extension Light Rail Transit (LRT) also known as the Bottineau LRT which will run through Brooklyn Park primarily along West Broadway. The Bottineau LRT is expected to be operational in 2022. Several County State Aid Highways are located across the City, providing connections with the adjacent communities. As increased development occurs, transportation improvements to the existing system will be needed to provide continuity, system capacity, and sufficient land access to developing areas. A more detailed analysis of the City's transportation network can be found in Chapter 5: Transportation.

3.2.3 Surrounding Communities

The City of Brooklyn Park covers approximately 27 square miles and is one of 45 cities in Hennepin County. The Mississippi River separates Hennepin from Anoka County. Brooklyn Park shares land borders with the following six cities: Champlin, Maple Grove, Osseo, New Hope, Crystal, and Brooklyn Center. Plymouth borders the southwest corner of Brooklyn Park, and Coon Rapids and Fridley share a border along the Mississippi River.

3.3 Existing Land Use

Existing land use information from the city’s GIS system and the Metropolitan Council is shown in [table 3-1](#) and [Map 3-3](#). Brooklyn Park’s largest land use category is its single-family detached neighborhoods which encompass over 42 percent of the land area of the city. A more detailed discussion of the various use categories follows.

TABLE 3-1: EXISTING LAND USE.

Existing Land Use in Brooklyn Park by total acreage and percent of total		
Land Use Type	Acres	Percent
Single Family Attached	865	5.1%
Single Family Detached	7186	42.3%
Multifamily	382	2.2%
Retail and Other Commercial	645	3.8%
Office	182	1.1%
Industrial and Utility	1081	6.4%
Railway	5	0.0%
Institutional Total	897	5.3%
Park, Recreational or Preserve	2005	11.8%
Golf Course	216	1.3%
Mixed Use Residential	3	0.0%
Mixed Use Industrial	62	0.4%
Mixed Use Commercial and Other	3	0.0%
Major Highways	939	5.5%
Airport	80	0.5%
Agriculture	835	4.9%
Undeveloped Land	1251	7.4%
Open Water	321	1.9%
Farmstead	20	0.1%
Total	16,978	100%

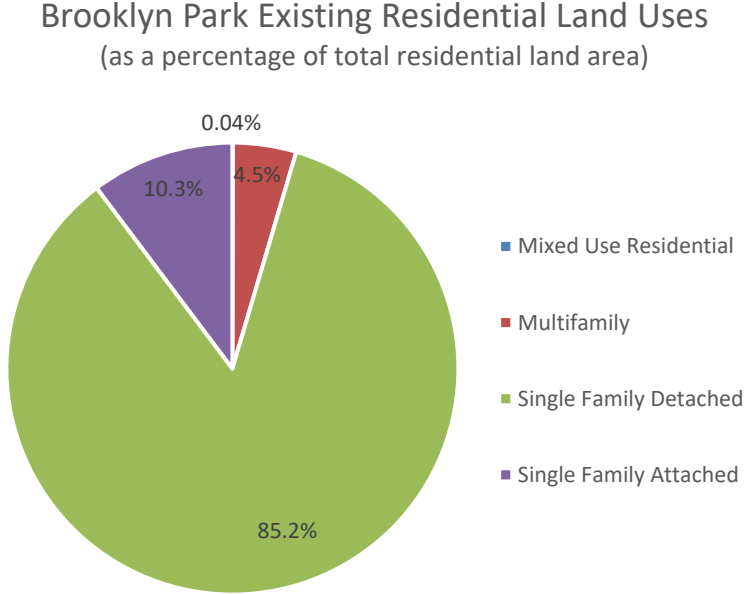
Source: Metropolitan Council 2016
Generalized Land Use

Existing Residential Land Use

The primary land use type in Brooklyn Park is residential of a variety of types. Nearly 50 percent of the city’s land is devoted to the combined residential uses of single family detached, single family attached housing, and multifamily housing. Drilling down further into the residential category, [figure 3.3-1](#) shows that single family residential housing units dominate the housing landscape in Brooklyn Park. Multi-Family residential

uses, which account for only 2.2% of the City’s land area, tend to be located in the southern part of the city along Zane Ave. N. and Bottineau Blvd.

FIGURE 3.3-1: EXISTING RESIDENTIAL USES (2016 METROPOLITAN COUNCIL GENERALIZED LAND USE)



Existing Retail and Other Commercial Uses

Retail and other commercial areas encompass approximately four percent of Brooklyn Park’s land area, with the largest commercial concentration along Bottineau Blvd between I-94 and 85th Ave. N. Other commercial nodes are located at key highway interchanges as well as smaller neighborhood-scale crossroads along 85th Ave. N., Brooklyn Blvd., and Brookdale Dr.

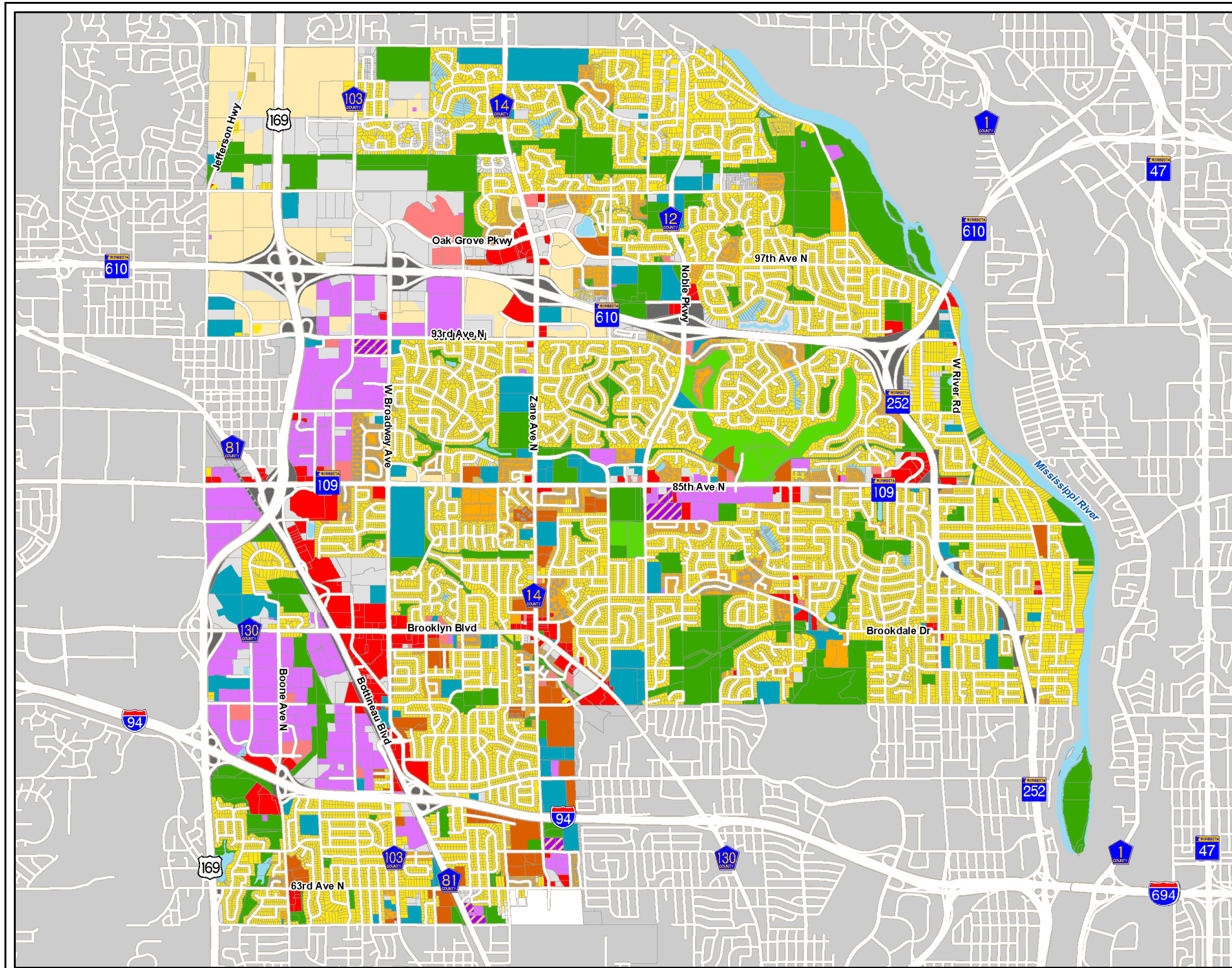
Existing Employment (Industrial and Office) Uses

Industrial and Office uses make up approximately 7.5 percent of Brooklyn Park, with industrial areas predominantly west of W Broadway Avenue and southwest of Bottineau Boulevard. Office complexes tend to be located in the same areas.

Parks, Open Space and Natural Features

Parks and Open Spaces play a large role in defining the sense of community in Brooklyn Park. Over 13 percent of its land area is devoted to parks, recreational or open space. Some of the largest include Coon Rapids Dam Regional Park, Oak Grove Park, and Edinburgh Golf Course.

Map 3-4 shows Brooklyn Park’s natural features, including floodplains (from FEMA) and wetlands (from the National Wetland Inventory). These natural features are an important part of the character of Brooklyn Park and are largely preserved in parks and public land areas. However, you can see that there are areas north of 610 that show wetlands. The areas have all been impacted by a change in the water table and wetland inventories completed for developments indicate that some of the wetland areas no longer exist.



Brooklyn Park

Brooklyn Park Comprehensive Plan

Existing Land Use
Brooklyn Park, MN

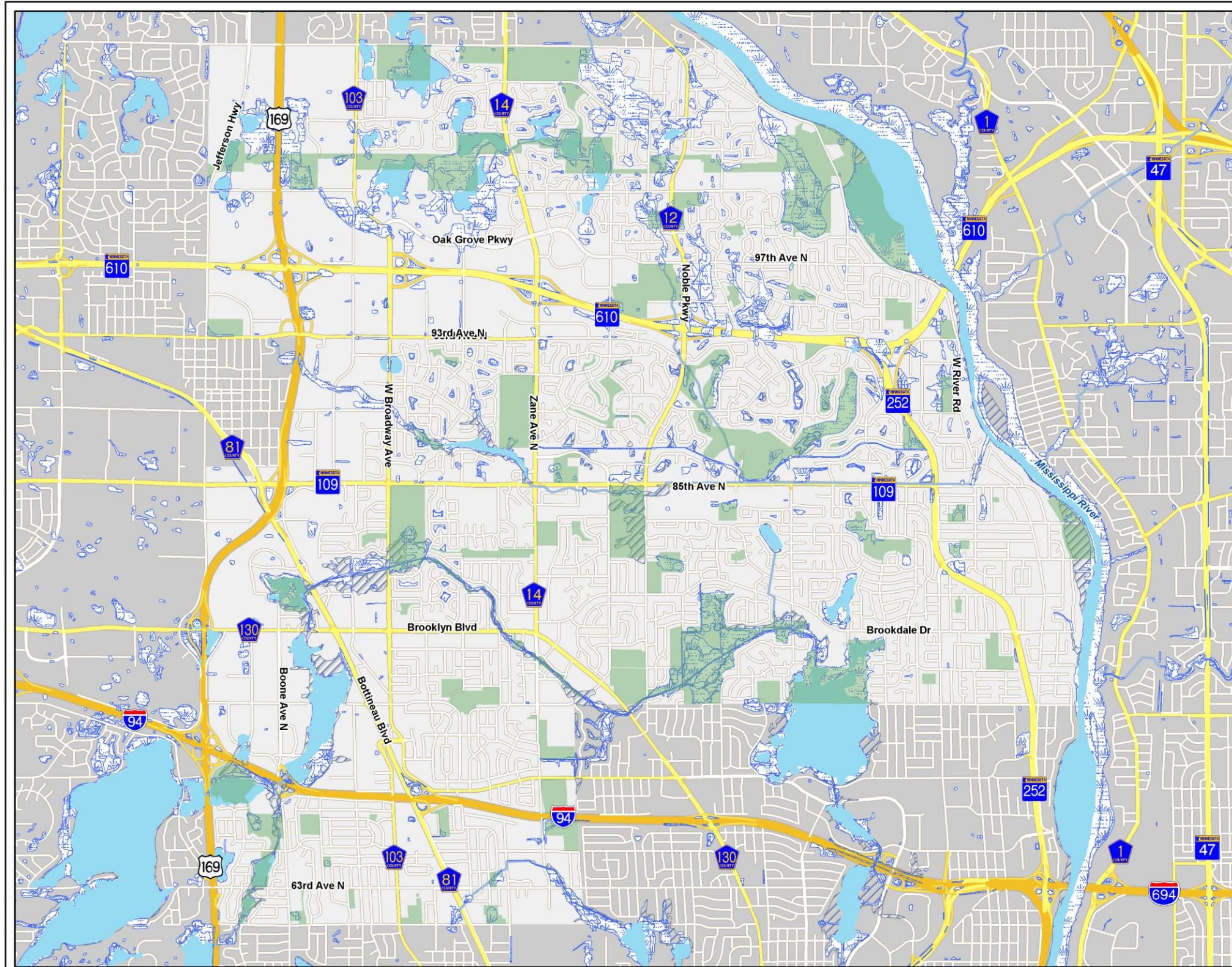
Existing Land Use Type

- Agricultural
- Airport
- Farmstead
- Golf Course
- Industrial and Utility
- Institutional
- Major Highway
- Major Railway
- Mixed Use Commercial
- Mixed Use Industrial
- Mixed Use Residential
- Multifamily
- Office
- Open Water
- Park, Recreational, or Preserve
- Retail and Other Commercial
- Single Family Attached
- Single Family Detached
- Undeveloped









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MAP 3-4: NATURAL FEATURES



**Brooklyn Park
Comprehensive Plan**

Natural Features
Brooklyn Park, MN

-  Wetlands
-  Public Waters Inventory
-  100-Year Floodplain
-  City Boundary
-  Parks
-  Streams and Rivers



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3.4 Regional Context: Community Designation

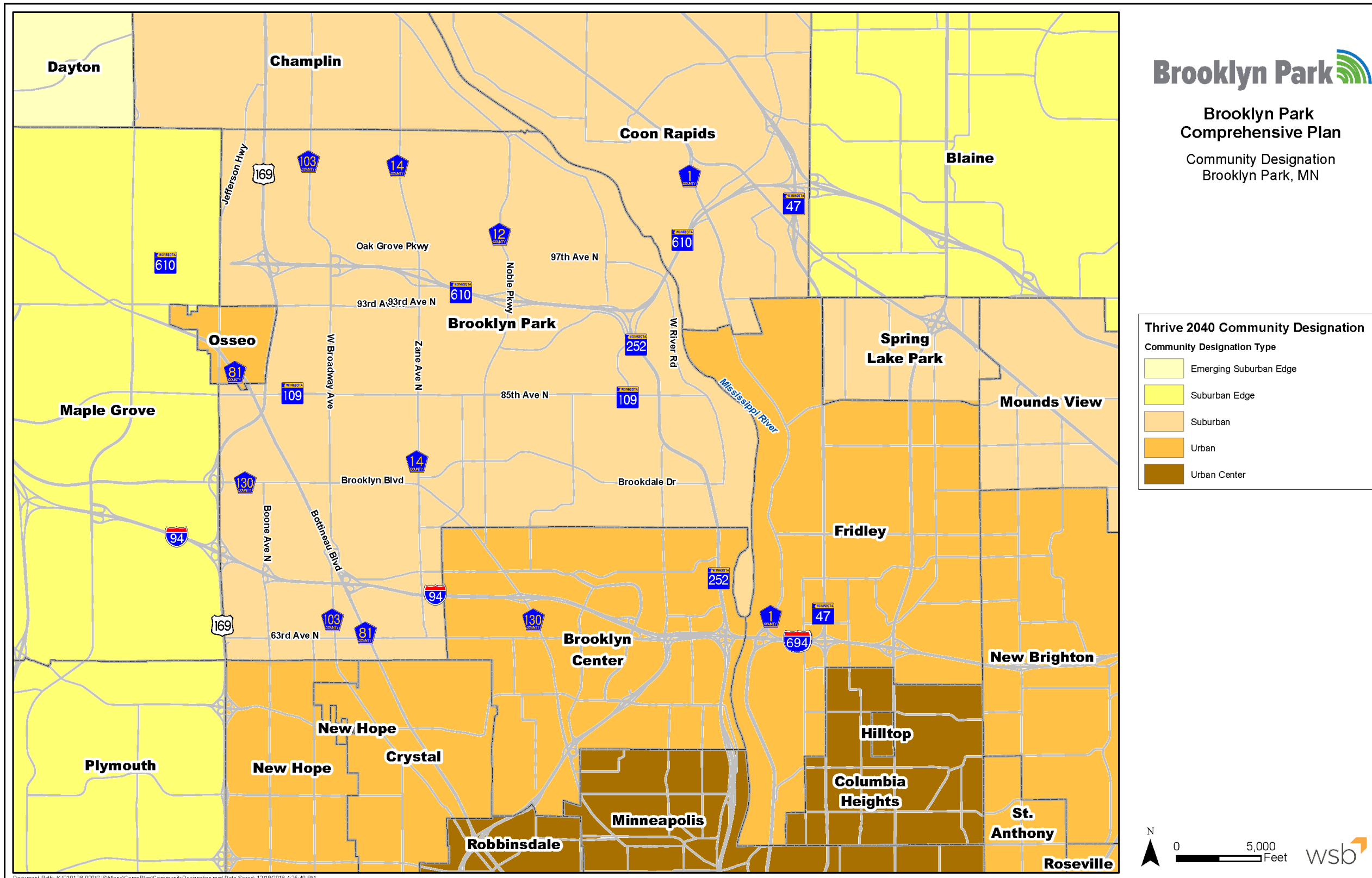
In Thrive MSP 2040 (“Thrive”), the Metropolitan Council groups jurisdictions within the region, based on each community’s existing development patterns, common challenges, and shared opportunities, for the application of the regional policies outlined in Thrive. As shown in [Map 3-5](#), Brooklyn Park’s “Suburban” designation translates to specific expectations in terms of development density and incorporation of transit services befitting its transitional location between the urban areas and less dense, more auto-dependent suburban-edge communities.

Comprehensive planning often involves planning for people and jobs that are not yet here. The Council develops population, housing, and employment forecasts as a component of a city’s System Statement to help set realistic goals and policies to plan for the future needs and trajectories of the community over the decades to come. The Council’s forecasts have been incorporated into this plan update.

Community Designation: The Metropolitan Council identifies Brooklyn Park as a Suburban community. Suburban communities experienced rapid development during the post-World War II era, and exhibit the transition toward the development stage dominated by the influence of the automobile.

Suburban communities experienced continued growth and expansion during the 1980s and early 1990s, and typically have automobile-oriented development patterns at significantly lower densities than in previous eras. Suburban communities are expected to plan for forecasted population and household growth at average densities of at least *5 units per acre* for new development and redevelopment. In addition, Suburban communities are expected to target opportunities for more intensive development near regional transit investments at densities and in a manner articulated in the *2040 Transportation Policy*

MAP 3-5: COMMUNITY DESIGNATION



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3.5 Expected Growth and Change

Future land use planning begins with incorporating forecasts of community growth and anticipating the needs that will arise as a result of this growth and change. The Metropolitan Council has developed growth forecasts for Brooklyn Park by decade, addressing the projected population, number of households, and number of jobs. Meeting expected growth projections requires intentional land use planning.

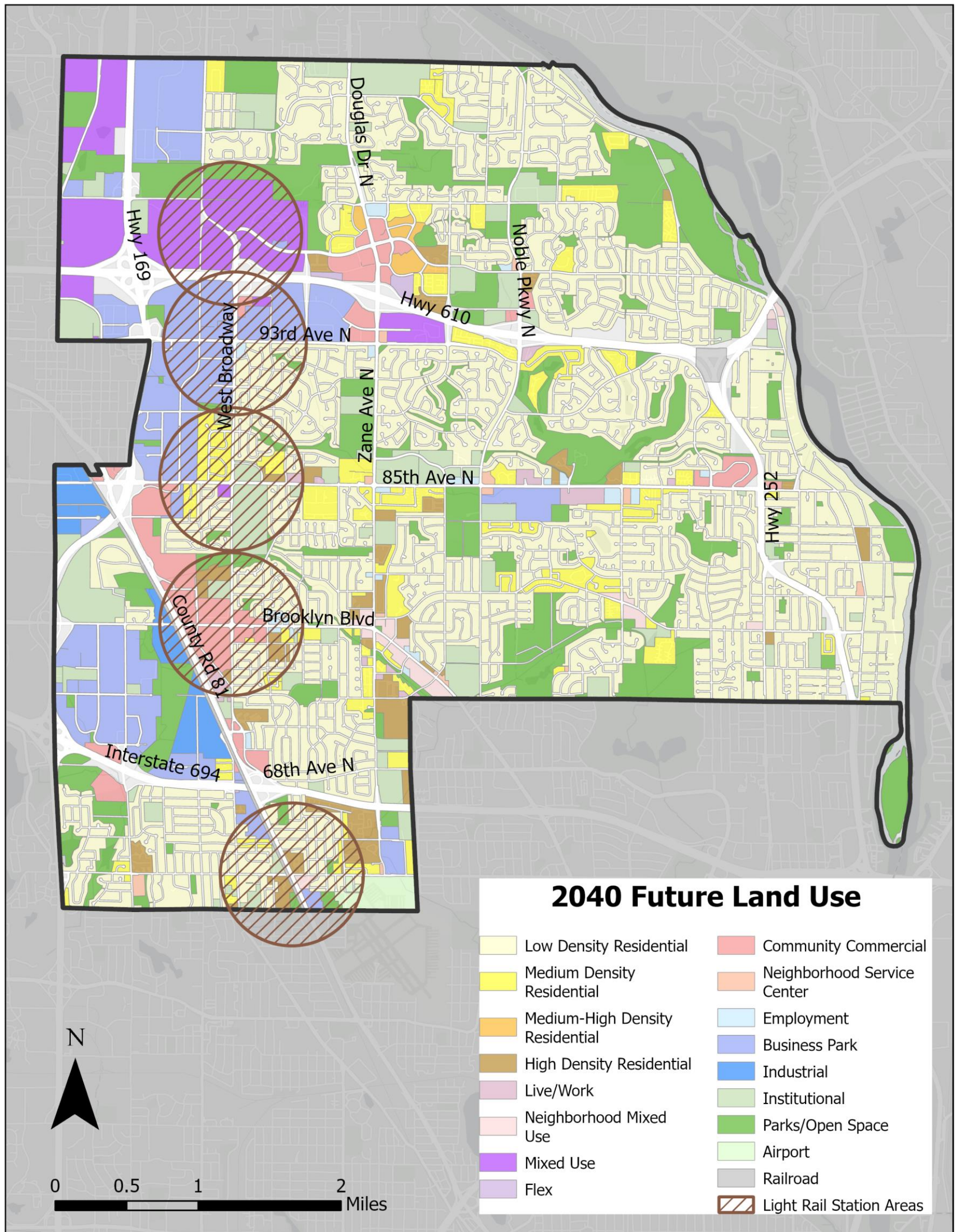
Forecast Year	Population	Persons per Household	Households	Employment
2010 (US Census Figures)	75,781	2.89	26,229	24,084
2016 (estimate)	80,450	2.92	27,539	--
2020	83,000	2.89	28,720	32,100
2030	90,000	2.85	31,400	36,100
2040	97,900	2.85	34,300	40,200
Overall Change from 2016	17,450	-0.07	6,761	16,116

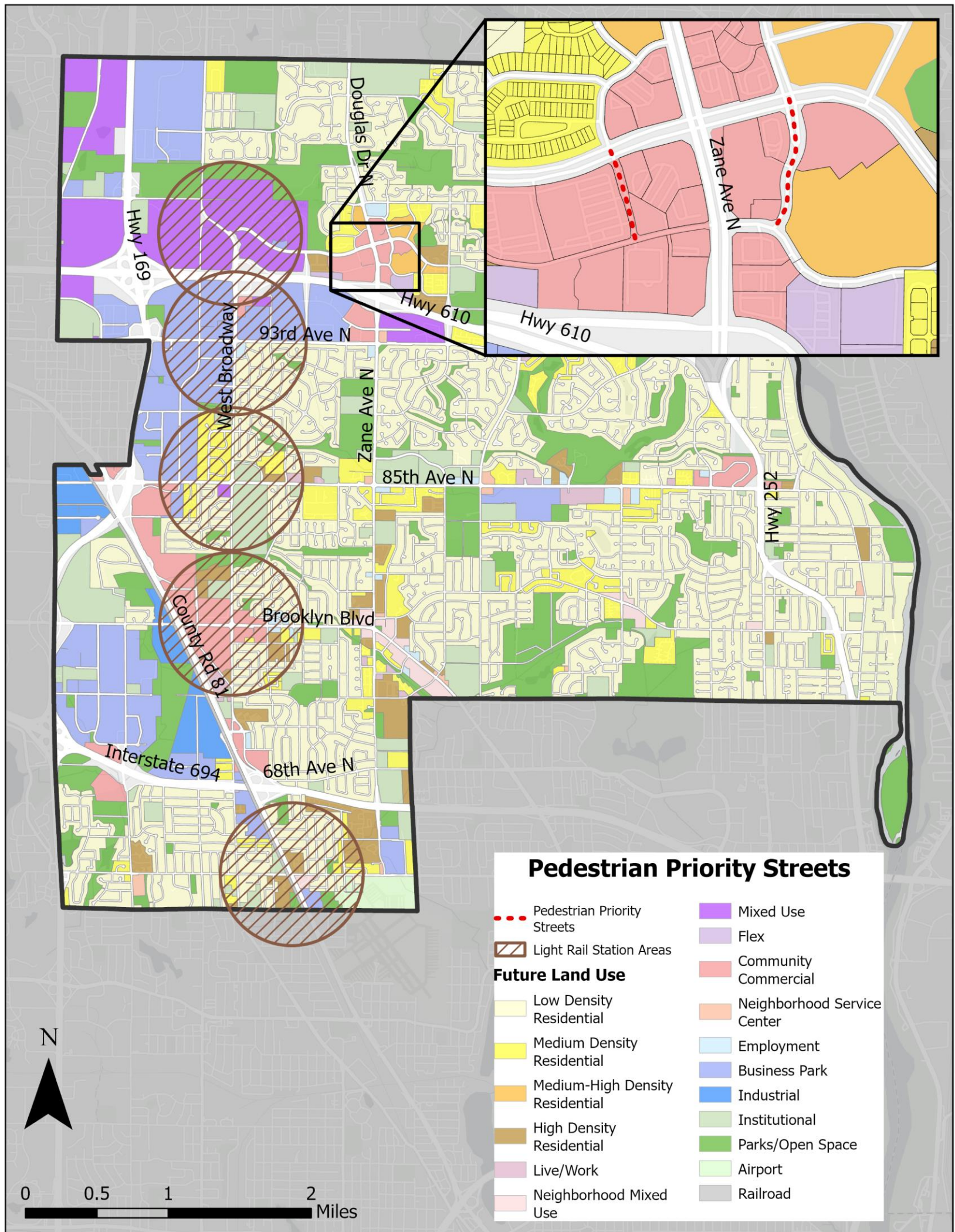
Source: Metropolitan Council

The original forecasts (from 2016) have been updated based on agreed upon revisions between City staff and the Metropolitan Council. Specifically, the 2020 numbers were revised to reflect trends in growth between 2010 and 2017. These forecasts show that the population is projected to increase in Brooklyn Park during the planning horizon by approximately 22 percent, while the number of households is expected to increase by almost 25 percent. This percent is consistent with the trend in decreasing household size; in Brooklyn Park the decrease is modest, with average household sizes going from 2.92 in 2016 to 2.85 in 2040.

3.5.1 Future Land Use Plan 2040

Map 3-6 illustrates the proposed future land use plan for the City to 2040, including areas that the City feels are likely to develop or redevelop before 2040. Following Map 3-6 are detailed descriptions of the existing and proposed land use definitions. The City uses the Future Land Use Plan to develop recommendations in areas of land use, supportive infrastructure, and development review. The Plan is specific enough to guide many day-to-day development decisions and provides the policies, standards, and principles that serve as the basis for updating the Zoning Ordinance and other development controls that the City enforces. The following goals and strategies were considered in the proposed alterations to the map.





Goals

Make the land use map a usable, reliable tool to plan and guide the future of development and redevelopment in the City

Make the map easy to read and understand

Ensure that the land use designations and definitions in the Comprehensive Plan are consistent with the visual map

Communicate clear and accurate planning of land uses to provide the development community with an understandable guide for future development.

Strategies

The following strategies are consistent with the Policy Objectives of the Comprehensive Plan. The strategies are meant to provide reasons and explanations for the proposed changes made to the Land Use map and district descriptions.

Sustainability: Focus pedestrian oriented commercial development in nodes rather than across long stretches of roadway to encourage close proximity and synergy between businesses. Proposed nodes conserve energy and resources and encourage vibrant commercial centers that will be sustainable into the future.

Neighborhoods: Connect neighborhoods to open space and amenities. Identify the different aesthetics and intensity of commercial development and properly locate appropriate commercial uses next to residential neighborhoods and/or address proper buffers between residential uses and high intensity commercial uses.

Commercial: Allow and encourage more mixed-use development, including both horizontal (a mix of uses in different buildings on the same site) and vertical (a mix of uses within the same building). Also de-emphasize the specific uses and building standards, leaving those for the zoning districts that will implement this future land use plan. The districts will emphasize the scale and intensity of the uses and the design as it relates to the surrounding area.

Employment and Services: Support healthy competitive commercial uses by focusing commercial growth at key intersections, identifying differences in commercial uses and choosing appropriate locations for differing commercial uses. Rewrite employment districts to de-emphasize specific uses and re-focus on the scale and intensity of development to bring more jobs to the community.

Open Space: Clearly identify community amenities such as open spaces, parks, water, historic landmarks and schools. Address conservation of natural resources such as water.

Transportation: Identify differences in commercial uses to better plan for special needs regarding transportation access and circulation. Encourage neighborhood commercial uses designed within close proximity to one another to allow the opportunity for Transit Oriented Development (TOD) that includes access by multiple forms of transportation including pedestrian, bus, and train.

Modifications

The following are descriptions of major modifications made between the 2030 and 2040 future land use plans:

Modification 1. Elimination of the Signature Mixed-Use District. This district has been combined with the Mixed-Use district. Uses in that area are now planned for Mixed Use, Business Park, or Medium-High Density Residential.

Modification 2. Change to the Neighborhood Commercial designation to Neighborhood Mixed Use to better reflect the intent of the district which allows for compact, pedestrian-oriented mixed-use areas of limited size. Include language on the scale and intensity of mixed use development in this district.

Modification 3. Density Changes to Residential Districts: Several changes were made to the density ranges for the four residential future land use districts. These were made to better reflect the pattern of development over the past decade and the types of residential development that the City projects will be in demand over the planning horizon. Minimum densities were added to each residential district and the maximum density were raised in the high density district from 25 units to 50 units per acre.

Modification 4. Introduction of a 30% minimum overall residential requirement in the Mixed-Use District. This minimum means that residential land uses will account for at least 30% of the overall Mixed Use District area, rather than 30% of any particular development site. The residential density range set for this district reflects the medium and high-density residential districts (12-50 units per acre).

Modification 5. Changes to the future land use map to reflect LRT Station Area Plans. These station areas include Oak Grove, 93rd Avenue, and 85th Avenue, Brooklyn Boulevard, and 63rd Avenue. The Station Area Plan was adopted in July, 2016 and specific overlay zoning in these areas is currently being developed. Minimum density for development within ½ mile of station areas is 20 units per acre.

Modification 6. Elimination of the “Commercial” district, which was added after the 2030 Plan was adopted and included all uses from any commercial district in the original 2030 Plan. The areas formerly in the Commercial district were reprogrammed as Community Commercial to reflect their existing use.

Modification 7. Renaming of the Office/Medium Density Residential district to Live Work. This change was made to more clearly indicate the intent of the district and add detail about the intended scale of new development and impact on surrounding neighborhoods. Specifics in the zoning ordinance will be provided on the intended design and scale of development in this district.

The complete description of future land use categories is included in [Table 3-2](#). The following definitions are meant to clarify the intent of the land use designations and provide a general vision of uses allowed in each designation. Added to each district is a summary indicating what type of scale and intensity is intended for the uses and structures there. Scale will be further described in the City’s Zoning Ordinance through bulk standards (height, setbacks, FAR, lot coverage); large scale uses have zoning parameters that allow more height, larger FAR, more lot coverage and in some cases reduced setbacks. Use intensity will be further defined in the zoning ordinance and relates to the types of uses permitted in a district. More “intense” uses include uses that generate more traffic, noise, impacts on public services and environmental impacts. In the case of residential uses intensity is often associated with density. Actual allowable uses and densities are

conveyed through the City's Zoning Ordinance. Transportation considerations include things like what sorts of connections are needed between buildings, uses, and major transportation routes. Multi-modal connections are often mentioned, which refers to pedestrian, bike, transit, and vehicular "modes" of transportation.

Note: there is no residential requirement in the Flex, Live/Work and Neighborhood Mixed Use districts. The City expects that district-wide there will be a mixture of residential and non-residential uses in these districts, but on a site-by-site basis there is no percentage requirement for residential or non-residential.

TABLE 3-2 FUTURE LAND USE DISTRICT DESCRIPTIONS

	Full Name	Summary	2040 Description
L	Low-Density Residential	<p><u>Density:</u> 1.5-3 units/acre</p> <p><u>Uses:</u> Single Family Residential</p> <p><u>Scale/intensity:</u> small scale, low intensity</p>	The low-density residential district accommodates single-family detached homes generally with a density between 1.5 and three units per acre.
M	Medium-Density Residential	<p><u>Density:</u> 3-12 units/acre</p> <p><u>Uses:</u> Single-family homes, condominiums, townhomes, duplexes, row houses and small apartment buildings</p> <p><u>Scale/intensity:</u> medium scale and intensity</p>	Medium-density residential land uses include smaller lot single family homes, single-family attached housing types such as triplex, quadruplex, row houses, townhomes, small-scale apartment buildings. Development densities are expected in the range of 3 to 12 units per acre.
MH	Medium-High Density Residential	<p><u>Density:</u> 12-25 units/acre</p> <p><u>Uses:</u> Apartments, lofts, stacked townhomes</p> <p><u>Scale/intensity:</u> medium to large scale and medium to high intensity</p>	This district is meant to be a “bridge” between the medium and high density residential districts, and includes an overlap of density ranges with each of those. Intended uses include very small lot detached and attached single-family housing such as those types described in the medium density district. Slightly larger apartment buildings may also be located here. Development densities are expected to be between 12 and 25 units per acre.
H	High Density Residential	<p><u>Density:</u> 12-50 units/acre</p> <p><u>Uses:</u> Apartments, lofts, stacked townhomes</p> <p><u>Scale/intensity:</u> medium to large scale and medium to high intensity</p>	The high-density residential district accommodates the highest density residential development in the city and includes multi-family owner and renter-occupied housing. Housing types may include (but are not limited to) townhomes, apartments, lofts, flats, and stacked townhomes. This district has densities that would also accommodate innovative housing arrangements such as tiny houses, co-housing, and others. Development densities are expected to be between 12 and 50 units per acre.
LW	Live/Work	<p><u>Density:</u> 3-12 units/acre</p> <p><u>Uses:</u> Medium-density residential, office, limited business</p> <p><u>Residential Requirement:</u> None</p> <p><u>Scale/intensity:</u> small-medium scale, low-medium intensity</p>	The Live/Work district accommodates a range of compatible small-scale, low intensity residential and employment uses. Housing is desired but optional for new development and should be in the density range of 3-12 units per acre. Uses may be mixed horizontally (on the same site, but not in the same building) or vertically (in the same building). Non-residential uses should include offices and limited businesses that have low impact with the residential uses. Buildings shall be scaled appropriately to the surrounding neighborhood, reflecting a low-to-mid-rise profile.

	Full Name	Summary	2040 Description
NMU	Neighborhood Mixed Use	<p><u>Density:</u> 9-50 units/acre</p> <p><u>Uses:</u> Medium-high-density residential, retail, office, service businesses</p> <p><u>Residential Requirement:</u> None</p> <p><u>Scale/intensity:</u> medium scale and intensity</p>	<p>The Neighborhood Mixed Use District is intended to provide for compact, pedestrian-oriented mixed-use areas of limited size designed for use in existing or potential pedestrian and transit nodes. Buildings shall be scaled appropriately depending on the type of street frontage (arterial, collector, neighborhood, etc). Higher profile buildings will be located along arterial or collector streets, with the scale, density and intensity of development scaling down as it approaches surrounding neighborhoods.</p> <p>The mix of uses should include residential at densities between 9 and 50 units per acre as well as retail, office and personal service establishments that are oriented to residents of the immediate neighborhood areas that anticipate high pedestrian traffic. Development in these areas shall follow a traditional neighborhood design where buildings are within a close proximity to the street and pedestrian connections are abundant. Both horizontal and vertical mixed use are encouraged in this district.</p>
MU	Mixed Use	<p><u>Density:</u> 12-100 units/acre</p> <p><u>Uses:</u> Medium-high- density residential, retail, service, office, restaurants</p> <p><u>Residential Requirement:</u> 30% minimum</p> <p><u>Scale/intensity:</u> Up to 8 stories (12 stories in LRT Overlay)</p>	<p>The Mixed Use District is intended to provide for pedestrian-oriented mixed-use development with a mixture of office, commercial, retail and residential uses. Development in these areas shall follow a traditional neighborhood design where buildings are within a close proximity to the street and pedestrian connections are abundant. The City encourages both horizontal and vertical mixed use, with residential densities between twelve and 100 units per acre. Housing must be an integral component of the overall development and will encompass at least 30% of the land mass of the district.</p> <p>The City will establish minimum development intensity standards to ensure efficient use of land and street design and require block dimension standards to promote walkability. Retail uses are required on properties fronting Pedestrian Priority Streets as designated on the Future Land Use map.</p> <p>The mix of uses may be in a common site, development area, or building. Individual developments may consist of a mix of two or more complementary uses that are compatible and connected to the surrounding area. To ensure that the desired development intensities and connections are achieved, a regulating plan is may be</p>

	Full Name	Summary	2040 Description
			required to guide phased development projects as governed by the Zoning Code.
F	Flex	<p><u>Density:</u> 7-25 units/acre</p> <p><u>Uses:</u> Office, limited business, commercial, medium-high-density residential</p> <p><u>Residential Requirement:</u> None</p> <p><u>Scale/intensity:</u> medium scale and intensity</p>	The Flex District is meant to provide the maximum amount of flexibility for future development. Zoning and development approval will depend on both the development of the surrounding area and market forces at the time a new development is proposed. The City recognizes that these sites could develop with a wide range of appropriate uses from medium-high-density residential to limited business.
NSC	Neighborhood Service Center	<p><u>Uses:</u> Small commercial, retail, office and service</p> <p><u>Scale/intensity:</u> small to medium scale and low to medium intensity</p>	Neighborhood Service Centers are small-scale commercial areas that are oriented to the surrounding residential areas. The businesses will be designed so that they are easy to reach by all modes of transportation, including nearby residents who may be walking or biking. Buildings shall be scaled appropriately to the surrounding neighborhood, reflecting a low-to-mid-rise profile.
CC	Community Commercial	<p><u>Uses:</u> Large format commercial, retail, and service</p> <p><u>Scale/intensity:</u> medium to large scale and medium to high intensity</p>	<p>Community Commercial areas are located on larger transportation thoroughfares and are of higher intensity/larger scale than other commercial areas in the city. Community Commercial districts are appropriate for large-format retail businesses and automobile-intensive uses that benefit from close proximity to highways.</p> <p>Design within the Community Commercial district should include appropriate sound and visual buffers between commercial and residential land uses.</p>
E	Employment	<p><u>Uses:</u> Small-scale office, business, research and some associated service and retail</p> <p><u>Scale/intensity:</u> small to medium scale and intensity</p>	Employment districts include a variety of smaller-scale office uses such as business, professional, administrative, medical, scientific, technical, research, and development services. Limited commercial may also be located in this district as it complements the employment uses.
BP	Business Park	<p><u>Uses:</u> Office, business, research and development, limited light industrial, limited retail, research and development, warehousing and distribution as part of a master plan.</p> <p><u>Scale/intensity:</u> medium to large scale and medium to high intensity</p>	Business Parks are largely single-use areas that have a consistent architectural style with a mix of employment-oriented use types. These uses may include office, mixed light industrial, limited retail, research and development, and uses of similar impact. With an increased focus on job creation, warehousing and distribution centers will be permitted if approved by the City as a part of a master plan. The scale of development in these areas is commensurate with their proximity to highways and

	Full Name	Summary	2040 Description
			major transportation corridors. Appropriate connections to transit should be included in Business Park developments.
I	Industrial	<u>Uses:</u> Manufacturing, light industrial, processing, warehousing, distribution <u>Scale/intensity:</u> medium to large scale and medium to high intensity	Uses within the Industrial districts may include manufacturing, assembly, processing, warehousing, distribution, related office uses. Industrial areas are appropriately located to minimize negative impacts on residential areas.
IN	Institutional	<u>Uses:</u> Civic, utilities, schools, places of worship, and public transportation facilities <u>Scale/intensity:</u> medium to large scale and medium to high intensity	Institutional land uses include civic, school, library, church, cemetery, and correctional facilities on a larger scale than those normally incorporated into the low-density residential area.
A	Airport	<u>Uses:</u> Airport	
P	Parks and Open Space	<u>Uses:</u> Parks, preservation areas, open space, golf courses <u>Scale/intensity:</u> small scale and low intensity	Parks and open space districts include public recreation areas (both active and passive), parks and privately owned preservation and natural areas.
LRT	LRT Overlay	<u>Density:</u> minimum 20 dwelling units/acre	The LRT Overlay district is located along the LRT corridor, and affects the mapped Development/Redevelopment Areas within a half-mile of LRT stations. Underlying primary zoning districts will govern land uses in these locations, except that any residential development occurring in the overlay must be at a minimum of 20 dwelling units per acre. Residential development should be well-connected to and accessible by those traveling by LRT line transit.
PPS	Pedestrian Priority Street		Pedestrian Priority Streets are located in areas designated Neighborhood Mixed Use or Mixed Use and serve as the main public focal points of each designated area. Streets must be designed to promote pedestrian and bicyclist safety and comfort, including ample sidewalk width, landscaping, and street furniture. New development is required to include retail fronting Pedestrian Priority Streets.

3.5.2 Future Land Use Planned Areas

The future land use plan presented in Map 3-6 includes the future land use districts and areas likely to develop or redevelop as discussed in previous sections of this chapter.

TABLE 3-3 FUTURE LAND USE PLANNED AREAS BY LAND USE CATEGORY¹

2040 Land Use Category	Total Acres	Percent of Total Land Area (%)
Airport	78	1%
Employment Center	1,293	10%
Community Commercial	439	3%
Flex	21	0%
High Density Residential	397	3%
Industrial	192	1%
Institutional	997	8%
Low Density Residential	5,626	43%
Medium Density Residential	862	7%
Medium-High Density Residential	49	0%
Mixed Use	697	5%
Neighborhood Mixed Use	88	1%
Neighborhood Service Center	34	0%
Employment	68	1%
Live/Work	69	1%
Parks and Open Space	2,239	17%
Total	13,148	100%

3.5.3 Meeting Growth Expectations

As stated earlier, Brooklyn Park’s “development-redevelopment areas,” are identified as opportunity areas for development or redevelopment within the 2040 planning horizon. Those areas are shown outlined and cross-hatched on Map 3-6 and are critical to understanding how the City meets its growth expectations for the coming decades.

Overall, the growth expectations for Brooklyn Park through the year 2040 can be met through the identified development/redevelopment areas in Map 3-6. Table 3-4 shows the residential acreage associated with the parcels included in the development/redevelopment areas, as well as the land use density ranges associated with each of these residential categories. Applying the expected density ranges to the land use areas, this table demonstrates the two ways in which the likely redevelopment areas will meet Brooklyn Park’s forecasted and expected 2040 growth:

- **Overall Average Density:** Applying the *minimum* end of the density range to each residential land use category, the overall density is the total number of expected minimum units divided by the total

¹ The difference between the future land use total (13,148) and the existing land use total from the 2016 Metropolitan Council data (16,978) is a total of the acreage in Major Highway (939), Railway (5), and Local Roads/ROW (2,886). These areas are not included on the 2040 Future Land Use Map as planned land use categories.

number of acres in the likely development/redevelopment areas. Brooklyn Park’s overall average density is 11.03 units per acre, which achieves the expected average for a Suburban community.

- **Overall Forecasted Growth:** Applying the *midpoint* of the density range to each residential land use category, the total number of expected units from this calculation is 7,977, which exceeds the 6,761 units forecasted growth from 2016 to 2040.

Note that a “30 percent” residential assumption is applied to the Mixed Use district as that is the amount of residential development *required* in that district, reducing both the guided land area and the potential residential units by 70-percent in that category.

The Flex and Live/Work Districts show a 10% yield for residential units because, while there is no minimum requirement for residential development on a site-by-site basis, the City expects that district-wide the percentage of residential will be approximately 10% in those districts. To summarize, the City’s *expectation* is that the mixed use districts with no required minimum residential will develop as follows:

Flex: 10% residential, 90% non-residential

Live/Work: 10% residential, 90% non-residential

Neighborhood Mixed Use: 10% residential, 90% non-residential

	Guided Land Use Type	Dev. Acres	Density Range			Yield %	Minimum Units (overall density and average)	Midpoint Units (meet forecast)
			Min	Mid	Max			
Guided in 2040 Plan	Low Density	21	1.5	2.25	3	100%	31	47
	Medium Density	3	3	7.5	12	100%	8	22
	Medium-High Density Residential	43	12	18.5	25	100%	516	801
	High Density	27	12	31	50	100%	324	833
	Live/Work	17	3	7.5	12	10%	5	13
	Flex Use	21	7	16	25	10%	14	33
	Neighborhood Mixed Use	31	9	29.5	50	10%	27	89
	Mixed Use	660	12	31	50	30%	2376	6139
Guided Total		299					3,301	7,977

Community Designation Density	
Overall Average	11.03

TABLE 3-4: RESIDENTIAL GROWTH POTENTIAL BASED ON FUTURE LAND USE DESIGNATIONS AND REDEVELOPMENT AREAS.

Looking once again at the likely development/redevelopment areas identified in the 2040 guide plan, Table 3-5, summarizes the commercial and employment development acreage in the city and translates this acreage into employment (jobs) potential. In total, development of the identified commercial, employment and mixed use areas could accommodate 18,071 new jobs in Brooklyn Park by the year 2040². This amount

² Space per employee assumptions based on average space requirements for employee by development type, Economic and Planning Systems Inc., 2016. FAR assumptions based on a sampling of FARs in Brooklyn Park and around the region.

exceeds the Met Council forecasted growth of 16,116 jobs between 2010 and 2040. Job creation is addressed in greater detail in the Economic Development chapter of this Plan.

Note that table 3-5 (in contrast to table 3-4) includes an expected percentage of non-residential development in the Live/Work, Flex and Neighborhood Mixed Use districts. As stated before, there is no *required* percentage in these district; however, the City *expects* to see these districts develop at approximately these percentages. Again, the percentages are district-wide and not on a site-by-site basis.

TABLE 3-5: POTENTIAL EMPLOYEE YIELD IN NON-RESIDENTIAL FUTURE LAND USE CATEGORIES

Future Land Use	Acres	Yield	F.A.R.	New Building Sq. Footage	Building Area Per Employee	Employee Yield
Live/Work	17.5	90%	20%	137,137	400	343
Flex	20.8	90%	20%	162,892	400	407
Mixed Use	660.3	70%	15%	3,019,922	400	7,550
Neighborhood Mixed Use	31.2	90%	20%	244,729	400	612
Neighborhood Service Center	1.0	100%	20%	8,759	400	22
Community Commercial	72.3	100%	20%	629,730	400	1,574
Employment	4.9	100%	20%	42,888	400	107
Business Park	339.5	100%	20%	2,957,731	400	7,394
Industrial	3.5	100%	20%	30,530	500	61
Total	1,151			7,234,317		18,071

3.6 Development Staging

Brooklyn Park has managed to avoid much semi-rural sprawl that might otherwise have occurred through its program of growth management and development staging. This section provides a general depiction of the historic development stages in the City and the plan for future growth.

3.6.1 Historic Development Staging

The following history of development staging in figure 3.2.4 shows the gradual development of the City since the 1940’s. The Village of Brooklyn Park was established in 1954. Early settlement in Brooklyn Park dates back to the early 1800’s. For example, the townsite of Harrisburg was platted in 1856, which included the area on the map, along the river labeled as developed between 1940 and 1960. Only one building from the original Harrisburg is still standing at 8900 West River Road. The house was reportedly used to house log boom workers between 1850-1880 (Brooklyn Park Historical Study).

3.6.2 Current Development Staging

Approximately 6% of the City or 793 gross acres of land is available for development in the remaining northwest growth area and various in-fill development areas throughout the City. There are also some strategic redevelopment sites that the City has identified, which add to the total acres available for development or redevelopment. Brooklyn Park is entirely within the Metropolitan Urban Service Area (MUSA) which identifies the area, in the seven counties, in which the Metropolitan Council ensures that regional services and facilities, such as sewers and major highways, are provided or planned. “A planning concept developed in the 1970s, the MUSA was designed to achieve orderly, economic and contiguous growth by directing development, primarily, to areas where roads and sewers already exist. The objective was to get the most use out of existing infrastructure and create efficiencies that save taxpayer dollars” (Metropolitan Council MUSA Fact Sheet).

MUSA is important because most cities’ staging plans are based on the extension of the MUSA boundary. However, Brooklyn Park is completely within the MUSA boundary, the current development staging plan in [Map 3-7](#) is meant to A) Allow flexibility in future development when proposed development meets the City goals and policy statements in the Comprehensive Plan and B) Provide information to citizens and the development community regarding the general infrastructure required for development to occur in the 2 development stages (Green and Red). The Green stage is encouraged to be developed prior to expansion into the other stage; however, the City will entertain development proposals in keeping with the City’s vision and goals and with consideration of available and planned infrastructure described below.

The Green stage represents property that has sanitary sewer and water available either at the property edge or within a feasible distance (Center of street or abutting property). The Green property also has access to reasonably adequate public roadways. It is likely that the areas shown in green will develop between 2021 and 2030.

Development between 2019-2020 is quite unpredictable given the uncertainties in the development review process. There are two sites, however, that the City can predict with some confidence will develop in the next year and a half. Those two are shown in Yellow on the staging map.

The Red stage generally represents property that does not have sanitary sewer and water available within a feasible distance and the surrounding public roadway system may be sub-standard depending on the uses proposed. These areas may be considered for development with a development proposal where all costs for utility extension are borne by the developer. The areas in red will likely be developed in the 2031-2040 timeframe.

[Table 3-6](#) shows the distribution of residential development over the planning horizon. The table demonstrates that the City has enough land programmed at appropriate density to support enough affordable housing units to exceed the allocation from the Metropolitan Council (2,213 units can be accommodated at a density of at least 12 units per acre where Brooklyn Park’s revised allocation, based on November 2018 forecast revision, is 710 units).

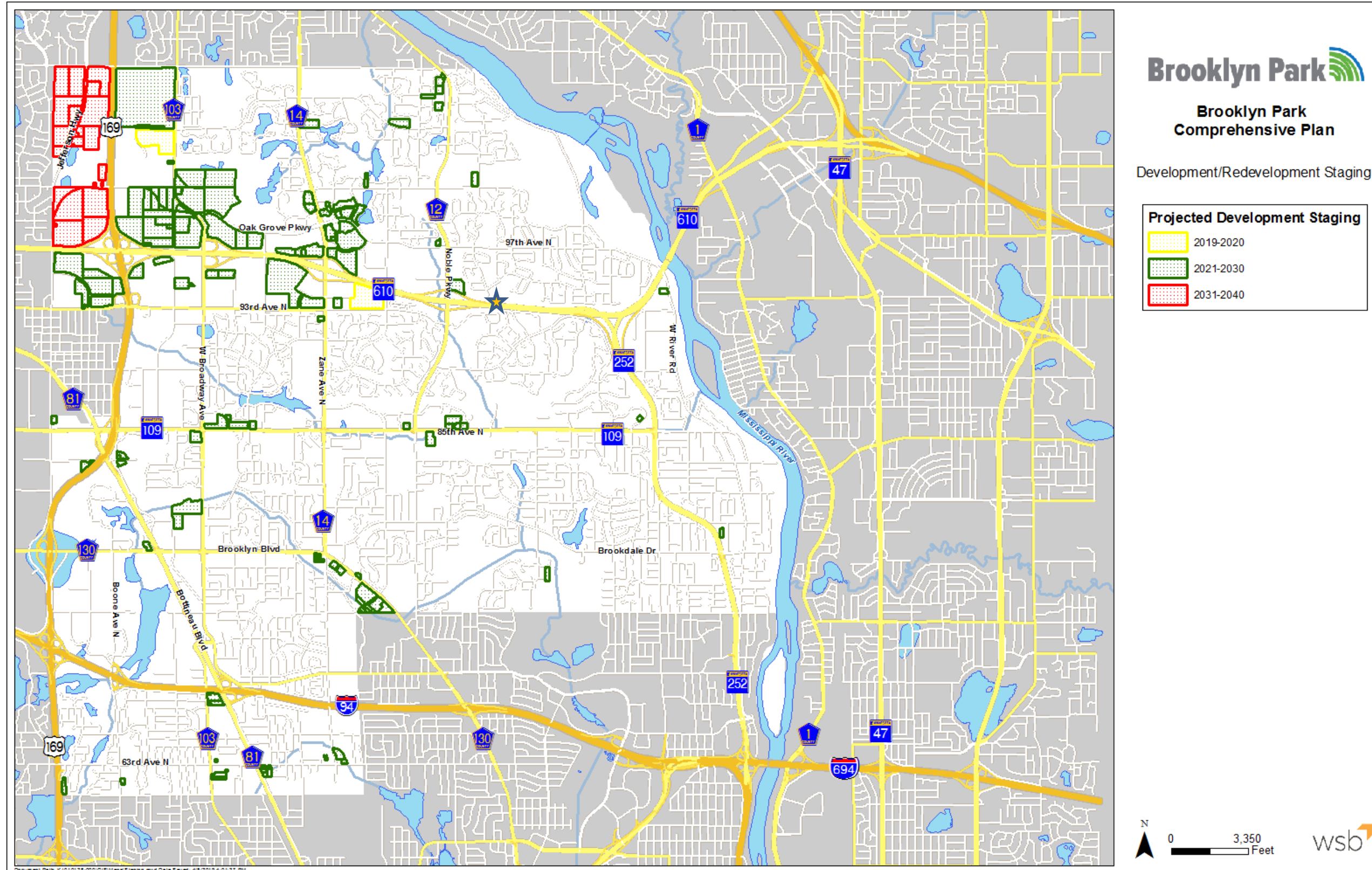


TABLE 3-6 DEVELOPMENT STAGING

	Guided Land Use Type	Dev. Acres	Now - 2020	2021- 2030	2031- 2040	Density Range			Yield %	Minimum Units now- 2020	Minimum Units 2021- 2030	Minimum Units 2031- 2040	Midpoint Units now- 2020	Midpoint Units 2021- 2030	Midpoint Units 2031- 2040
						Min	Mid	Max							
Guided in 2040 Plan	Low Density	21	0	21	0	1.5	2.25	3	100%	0	31	0	0	47	0
	Medium Density	3	0	3	0	3	7.5	12	100%	0	8	0	0	22	0
	Medium-High Density Residential	43	0	43	0	12	18.5	25	100%	0	516	0	0	801	0
	High Density	27	0	27	0	12	31	50	100%	0	324	0	0	833	0
	Live/Work	17	0	17	0	3	7.5	12	10%	0	5	0	0	13	0
	Flex Use	21	0	21	0	7	16	25	10%	0	14	0	0	33	0
	Neighborhood Mixed Use	31	0	15	16	9	29.5	50	10%	0	14	13	0	44	45
Mixed Use	660	35	378	248	12	31	50	30%	125	1359	892	321	3512	2306	
Guided Total		299	35	525	248					125	2,271	905	321	5,305	2,351
SUM TOTAL (2040 Guided)		299								3,301			7,977		

Density Table	
Overall Community Density	11.03
Expected Housing Unit Growth	7,977
Units Considered Affordable (≥12 du/ac in Pre-2030)	2,213

3.7 Procedures to Amend Land Use Designation

If an alternative use is proposed that compromises a land use definition above, then a Comprehensive Plan Amendment shall be considered for the property. The Procedures for application and public hearing of Comprehensive Plan Amendments are described in Section 152.031 of the City Zoning Ordinance. Issues to consider regarding a Comprehensive Plan Amendment include but are not limited to: Land Use Goals and Policy Statements in the Comprehensive Plan, City policy changes, transportation conditions, changes in the marketplace, environmental concerns and changes in the surrounding neighborhood. Economic considerations may not constitute a change in Land Use if reasonable use of the property exists within the current land use designation.

3.8 Two or More Land Uses on One Parcel

In the case that one land parcel includes two or more land use designations, a blending of the land uses may be permitted with a master development plan. The lines distinguishing the different land uses are not meant to be literal, but rather a general guide of mixed land uses. Issues to consider when reviewing a blending of the land uses may include, but are not limited to, the following: The surrounding land use designations, existing uses, transportation network and natural environment.

3.9 Station Area Planning

Brooklyn Park will be home to five of the 11 light rail stops on the METRO Blue Line Extension Light Rail Transit (LRT), often called the Bottineau LRT. The Bottineau LRT corridor extends approximately five miles north/ south through Brooklyn Park along the County Road 81 and West Broadway Avenue corridors. The vision for the station areas is to reinforce and strengthen the unique characteristics of each of the

neighborhoods surrounding the five stations. The Station Area Plans identify infrastructure improvements, redevelopment options, and opportunity sites within a ½-mile of each stop. The Brooklyn Park Station Area planning process was a joint effort of Hennepin County and the City of Brooklyn Park. The process began in spring 2015 and concluded with the publication of the plans in summer 2016.

The complete Brooklyn Park Station Area Plan document can be found in Appendix H.

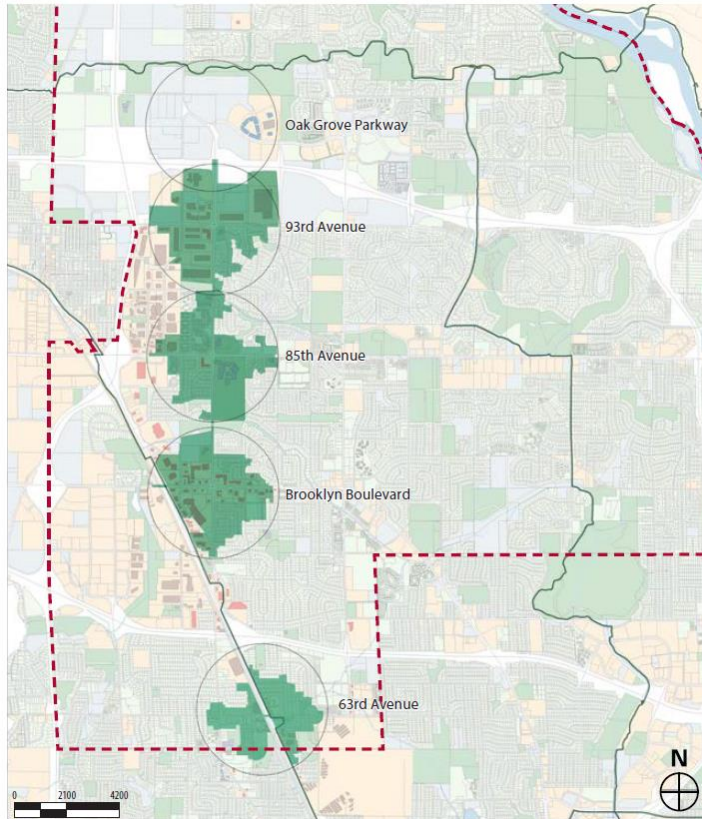


Fig. 3 10-MINUTE WALKSHED

MAP 3-8 STATION AREAS AND 10-MINUTE WALK RADII (BROOKLYN PARK STATION AREA PLAN)

3.9.1 63rd Avenue Station

The focus of the 63rd Avenue station area plan is the intersection of County Road 81 and 63rd Ave. The area around the station is mainly defined by low-density residential neighborhoods encircling a node of higher intensity uses at this intersection. In addition to Transit Oriented Development Overlay Zoning, primary highlights for the station area plan include:

- a local ethnic market place or pop-up market space
- long-term redevelopment sites
- pedestrian enhancements at the intersection

MAP 3-9 63RD AVE. STATION AREA PLAN



- | | | |
|--|--|---|
|  TOD OVERLAY |  INSTITUTIONAL BUILDING | 1 Local Ethnic Marketplace or Pop-Up Market Space |
|  STATION |  PUBLIC PARKS AND OPEN SPACE | 2 Long-Term Redevelopment Sites |
|  SINGLE-FAMILY RESIDENTIAL BUILDING |  RESIDENTIAL LAND | 3 Transit Oriented Development Overlay |
|  MULTI-FAMILY RESIDENTIAL BUILDING |  COMMERCIAL AND INDUSTRIAL LAND | 4 Pedestrian Enhancements at Intersection (being studied) |
|  COMMERCIAL BUILDING | | |

3.9.2 Brooklyn Boulevard Station Area Plan

The Brooklyn Boulevard station, in contrast to the 63rd Avenue station, is located in a bustling commercial environment. Shopping centers and businesses of varying sizes and intensities predominate. Low density residential uses are located to the northeast and southeast. It is anticipated that the light rail line will strengthen the overall commercial environment while also supporting new uses over time, including more housing. Highlights of this station area plan include:

- Pedestrian connections
- Transit Oriented Development Overlay
- Short-term redevelopment sites

3.9.3 85th Avenue Station Area Plan

North Hennepin Community College (NHCC) occupies the eastern half of the 85th Avenue Station area and the Hennepin County Branch Library is here as well. It is anticipated that the LRT will be a catalyst for redevelopment associated with the NHCC's projected growth. Highlights of this station area plan include:

- Hennepin County Brooklyn Park Branch Library
- North Hennepin Community College Master Facilities Plan (2015)
- Civic Plaza
- Long-Term Redevelopment Sites

3.9.4 93rd Avenue Station Area Plan

This area encompasses a large business park within Brooklyn Park. The LRT stop brings the need for enhanced pedestrian and bike connections. Primary initiatives in the station area plan include:

- Improved pedestrian connections
- Incentivize transit oriented development

MAP 3-10 BROOKLYN BOULEVARD STATION AREA PLAN



FIGURE 3-10 BROOKLYN BOULEVARD STATION AREA PLAN

- ★ APPROXIMATE STATION LOCATION
- STATION
- MIXED-USE BUILDING
- HIGH-DENSITY RESIDENTIAL
- PUBLIC PARKS AND OPEN SPACE
- COMMERCIAL AND INDUSTRIAL LAND
- /// TOD OVERLAY
- 1 Pedestrian Connections
- 2 Transit Oriented Development Overlay
- 3 Short-Term Redevelopment Sites

MAP 3-11 85TH AVE. STATION AREA PLAN



Fig. 42 ILLUSTRATIVE PLAN FOR 85TH AVENUE STATION AREA

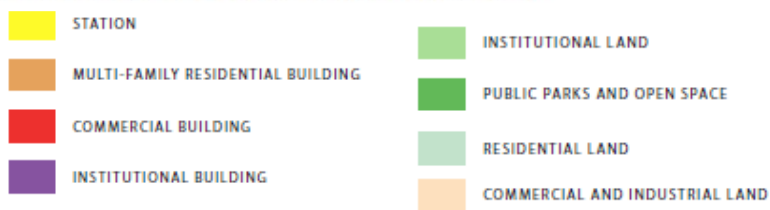
- ① Hennepin County Brooklyn Park Branch Library
- ② North Hennepin Community College Master Facilities Plan
- ③ Civic Plaza
- ④ Long-Term Redevelopment Sites

 STATION	 INSTITUTIONAL LAND
 MULTI-FAMILY RESIDENTIAL BUILDING	 PUBLIC PARKS AND OPEN SPACE
 COMMERCIAL BUILDING	 RESIDENTIAL LAND
 INSTITUTIONAL BUILDING	 COMMERCIAL AND INDUSTRIAL LAND

MAP 3-12 93RD AVE. STATION AREA PLAN



Fig. 44 ILLUSTRATIVE PLAN FOR 93RD AVENUE STATION AREA



3.9.5 Oak Grove Station Area Plan

Finally, the Oak Grove station is the northernmost on the Bottineau LRT line. Target Corporation owns the bulk of the land surrounding this station and has constructed the first phase of its Northern Campus just to the east of West Broadway Ave. The LRT represents a direct link between this facility and Target's Downtown Minneapolis Headquarters.

Other key land uses in this area include the Rush Creek Regional Trail to the north (and City-owned open-space abutting the trail), a planned operations and maintenance facility for the LRT, and a large church. The site also has easy highway access to both 610 and 169. A park-and-ride facility is also planned for this area.

This site represents the largest development opportunity site along the Bottineau LRT and in the region as a whole. Highlights of this station area plan include:

- Parking ramp and TOD liner
- Street network
- Grand boulevard
- Future development



OAK GROVE STATION: GRAND BOULEVARD ADDRESS ON WEST BROADWAY AVENUE

MAP 3-13 OAK GROVE STATION AREA PLAN



Fig. 46 ILLUSTRATIVE PLAN FOR OAK GROVE STATION AREA



3.10 Building to 2030

The City conducted a special area study in 2013 called Building to 2030. The work was completed by a task force, is still relevant today and complements the 2040 Comprehensive Plan. Following is the Executive Summary from the report and the policies that are in place to help guide development in the Highway 610 corridor and surrounding areas. The entire report is available in the planning department.

Building to 2030 Executive Summary

On June 10, 2013, the City Council authorized the formation of the Signature Development Task Force to help define the community's desired outcomes regarding future development in Brooklyn Park. The role of the Task Force was to develop citizen driven policy recommendations for the City Council and Economic Development Authority (EDA) that can be used to guide future development in Brooklyn Park. The Task Force's policy recommendations cover a wide range of desired development outcomes including job creation, tax base enhancement, long-term financial viability, quality design, housing, transportation, green space, recreation opportunities, community gathering destinations, and opportunities for retail. The Task Force's recommendations will inform the City Council and EDA's future economic development and infrastructure investments, use of financial incentives, changes to land use regulation, and bring to light innovative approaches to land use in northwestern Brooklyn Park.

The Task Force began the visioning process by first agreeing on themes that captured the community's wants and needs for future development. The themes were based on the feedback from the May 1, 2013 work session held with City Council, Planning Commission, Citizen Long-range Improvement Committee, and Recreation and Parks Advisory Committee regarding the future of development in northwestern Brooklyn Park:

- Attention to Nature
- Jobs! Jobs! Jobs!
- Many Housing Options
- Integrated Ways to Get Around
- Flexible Framework for Intentional Growth
- Distinct and Inviting Community Places
- Design Matters!
- Food and Fun

The vision themes drove the work plan and for the Task Force which was aided by staff from the Brooklyn Park Community Development Department. From August 1 to December 12, ten meetings were held addressing the vision themes through education and discourse using Staff's expertise, presentations by outside experts, panel discussions, a design tour, and interactive exercises.

Forming a clear vision was only the first step toward consensus building amongst the Task Force. Though exposed to the same information throughout the process, the Task Force members had individual reactions and opinions on the various topics. To capture ideas and reactions at each meeting, staff facilitation discussion of group insights, essentially: "what are your takeaways from the meeting?", "what did you learn?" and "what do we need to remember moving forward?" The goal was to collect these reactions, as

“meeting takeaways” and present them back to the group at the following meeting more formally as “insights”. By giving the group time to reflect on and discuss insights, the Task Force was able to produce recommendations that stayed true to the vision and group supported insights.

Drawing from their personal knowledge of the community, experience on various boards and committees, and the information presented to them by staff and outside experts, the Signature Development Task Force successfully formed policy recommendations for the City Council’s consideration.

The insights, recommendation, and implementation ideas are presented in detail starting on page 19 of the report. The following represent the final policy recommendations presented by the Signature Development Task Force to City Council and the EDA:

Attention to Nature and Recreation

- ❖ Plan for passive and active green spaces around activity areas and residential neighborhoods.
- ❖ Create a more desirable and amenity rich environment with quality landscape design and maintenance because integrated green spaces are a valuable asset to the community.

Jobs! Jobs! Jobs!

- ❖ Consider providing assistance only to developments that promise a return on the City’s investment: high use, high employment, high skill and/or high tech.
- ❖ Streamline and simplify the approval process for businesses and development that meet our vision; be fast, flexible, and friendly..
- ❖ Continue emphasis on attracting precision manufacturing, biomedical, corporate, and other high-tech industries along the Highway 610 corridor.
- ❖ Expect job-rich development, whether or not projects include public funds, so that the community can reach a goal of 50,000 jobs by 2030.

Many Housing Options

- ❖ Support high quality, sustainable multi-family developments by planning for locations near jobs and activity centers.
- ❖ Support multi-family developments with sustainable characteristics with preference for complexes that provide unit variety, amenities, underground/structured/attached parking, proven management capacity, and involved ownership.
- ❖ Attract high quality multi-family developments to serve new jobs and changing demands for housing types.

Integrated Ways to Get Around

- ❖ Use strategic transportation investments and well-planned transportation improvements to improve access to and from Brooklyn Park.

- ❖ Be open to opportunities for transit use, transit efficiency, and walkability for residents and visitors.
- ❖ Leverage regional transportation investments.
- ❖ Ensure that parking for transit hubs are planned to mitigate negative impacts to surrounding neighborhoods.

Flexible Framework for Intentional Growth

- ❖ Have a unified vision to reduce uncertainty for developers and keep the community on its desired path.
- ❖ Be deliberate in realizing development that meets our vision: balance the desire for immediate development with the long range vision.
- ❖ Support development that uses land effectively and is consistent with the vision.
- ❖ Streamline and simplify the approval process for businesses and development that meet our vision; be fast, flexible, and friendly.
- ❖ Partner with Target to coordinate planning and development on surrounding undeveloped land.

Distinct and Inviting Community Places

- ❖ Partner with Target to develop a destination spot that puts Brooklyn Park on the map.
- ❖ Encourage developments to incorporate distinct indoor and outdoor public spaces. Consider recreation, gathering areas, design, art, and iconic elements to create special places in Brooklyn Park.

Design Matters!

- ❖ Rely on design principles to create a distinct identity and promote a stable financial future for Brooklyn Park.
- ❖ Promote exceptional design and signature buildings along the TH 610 corridor.
- ❖ Encourage efficient use of land through structured parking.

Food and Fun

- ❖ Plan for a mix of retail, entertainment, and housing that creates a thriving neighborhood for residents and a destination for visitors.
- ❖ Encourage public spaces with indoor and outdoor recreation, parks, gathering areas, and restaurants.
- ❖ Encourage balanced development that creates a market for desired restaurants, retail and other amenities.

The report will take the reader through this citizen driven planning process with special attention to the Task Force’s visioning and experience leading to the policy recommendations. To further explore the process and material presented, please refer to the appendices for agendas, meeting notes, presentations, and other informational material.